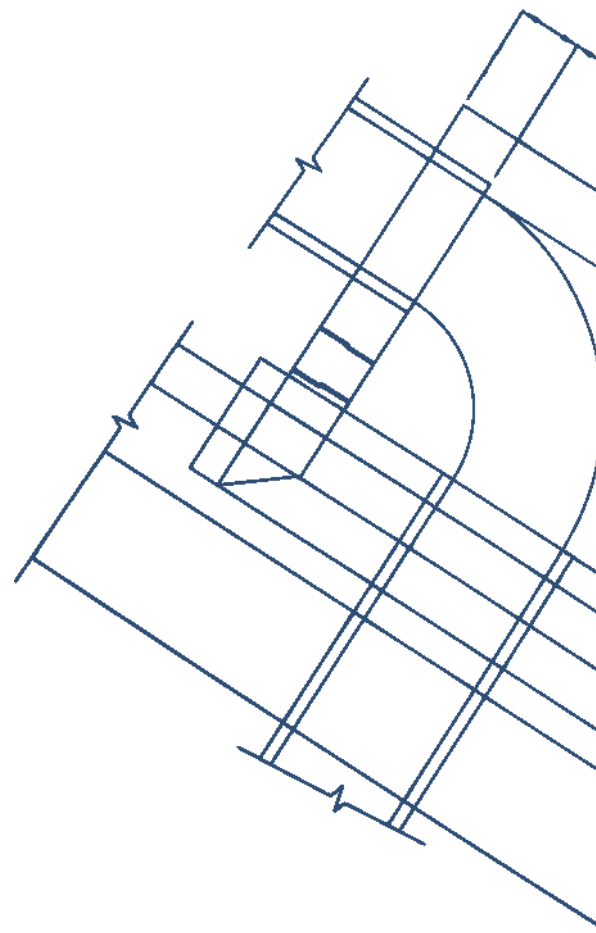
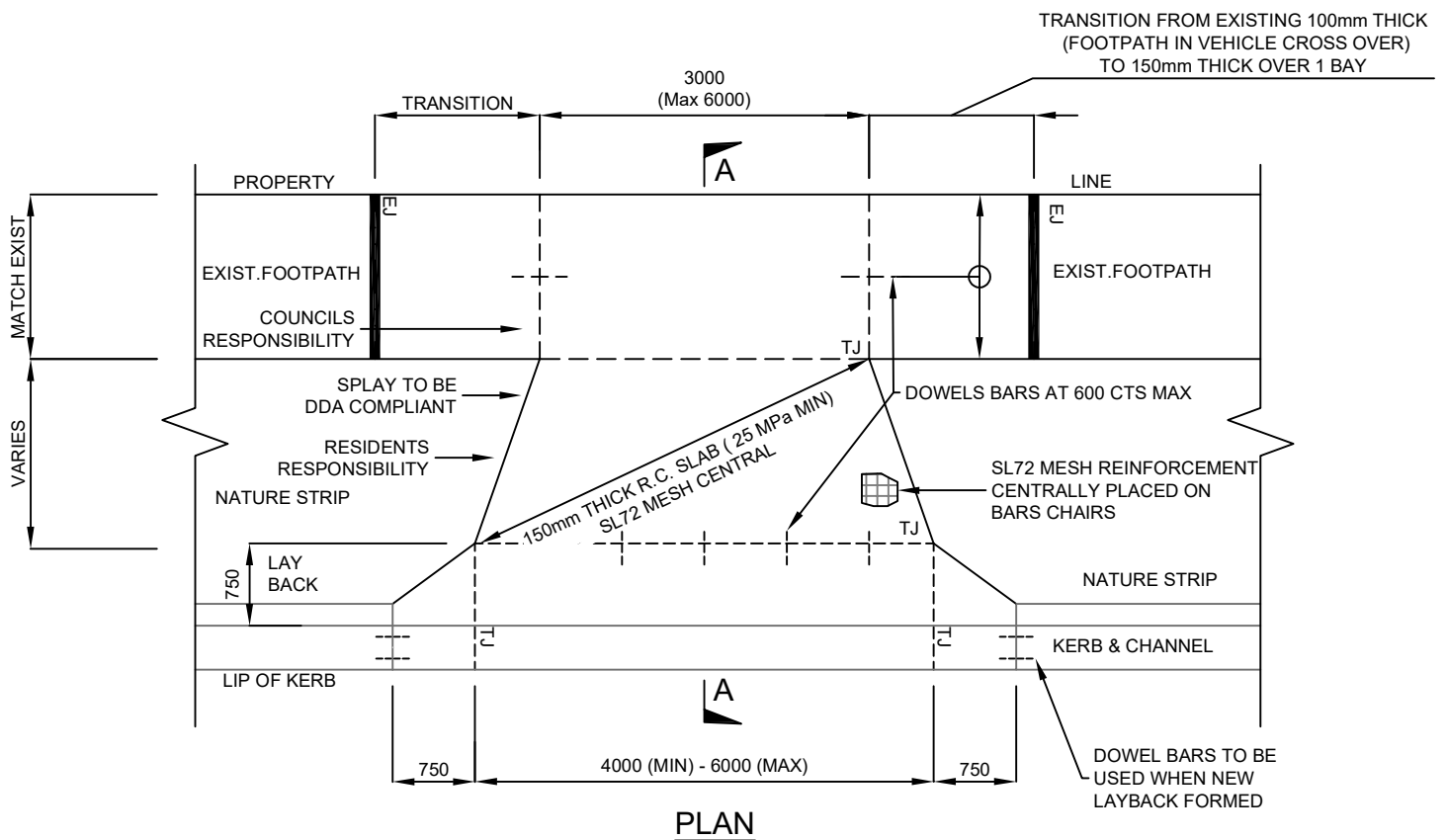


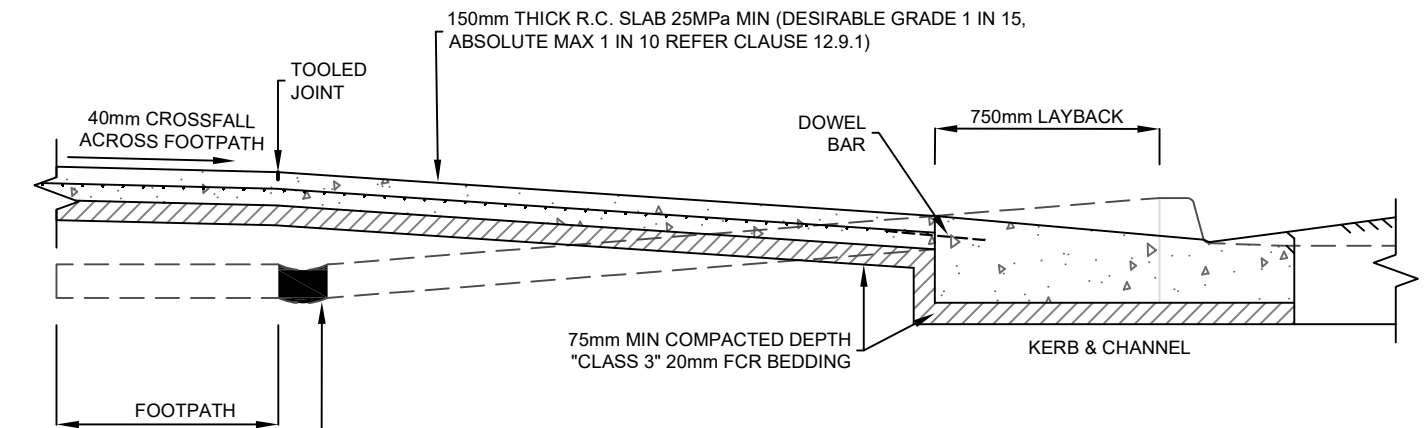
# Vehicle Crossovers



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**PLAN**



**CROSS SECTION A-A**

**LEGEND**

- EXPANSION JOINT EJ
- TOOLED JOINTS TJ

IF REVERSE FALL IS REQUIRED DESIGN OF VEHICLE CROSSING TO BE ON A SITE SPECIFIC BASIS

**NOTES:**

1. THIS DRAWING DETAILS DIMENSIONS FOR STANDARD RESIDENTIAL CROSSINGS ONLY.
2. CROSSING WIDTHS EXCEEDING THE MAXIMUM ALLOWABLE WILL REQUIRE APPLICATION FOR SPECIAL CONSIDERATION.
3. JOINTS AND DOWEL BARS ARE REQUIRED ON EITHER SIDE OF THE CROSSING AT THE INTERFACE WITH THE FOOTPATH. PROVISION SHALL BE MADE IN EXISTING CONCRETE SECTIONS BY DRILLING HOLES TO A MINIMUM DEPTH OF 150mm AND INSERTING R12 X 300mm LONG DOWEL BARS.
4. AN APPROVED JOINT FILLER SHALL BE PLACED ON EITHER SIDE OF THE CROSSING AGAINST FOOTPATH SLABS. DOWEL BARS ARE TO HAVE AN APPROVED BOND BREAKER APPLIED TO THE END OF THE BAR INSERTED INTO THE EXISTING CONCRETE FOOTPATH SECTIONS REFER A 155.
5. ADDITIONAL TOOLED JOINT REQUIRED IF DISTANCE FROM BACK OF KERB TO FOOTPATH IS GREATER THAN 3000 AND SHALL BE PLACED AT THE MIDPOINT OF THE DISTANCE.
6. THE MAXIMUM NUMBER OF CROSSINGS, WHERE ANY CROSSING EXCEEDS 3.5 METRES WIDTH, SHALL BE ONE (1) CROSSING WITH THE MAXIMUM WIDTH OF THAT CROSSING TO BE 6.0 METRES. CROSSINGS TO ADJACENT PROPERTIES SHALL BE EITHER FULLY COMBINED, AND OF MAXIMUM WIDTH OF 6.0 METRES, OR ELSE HAVE A MINIMUM SEPARATION OF 9 METRES.
7. FOOTPATHS OF 100mm THICKNESS ARE ACCEPTABLE ONLY WHERE THE LOTS ARE DEVELOPED ALREADY AND THE RISK OF SITE CONSTRUCTION DAMAGE IS NEGLIGIBLE. WHERE GREENFIELD SITES AND FUTURE HOUSING IS STILL TO BE DONE, THEN THE DEPTH OF THE FOOTPATH SHALL BE 150mm THROUGHOUT.
8. AS DEFINED BY LOCAL LAWS PART 4 AND THE ROAD MANAGEMENT PLAN SECTION 2, CONSTRUCTION AND MAINTENANCE OF FOOTPATH IS COUNCILS RESPONSIBILITY.

ALL MEASUREMENTS IN MILLIMETRES



**RETROFIT RESIDENTIAL VEHICLE CROSSING DETAIL**

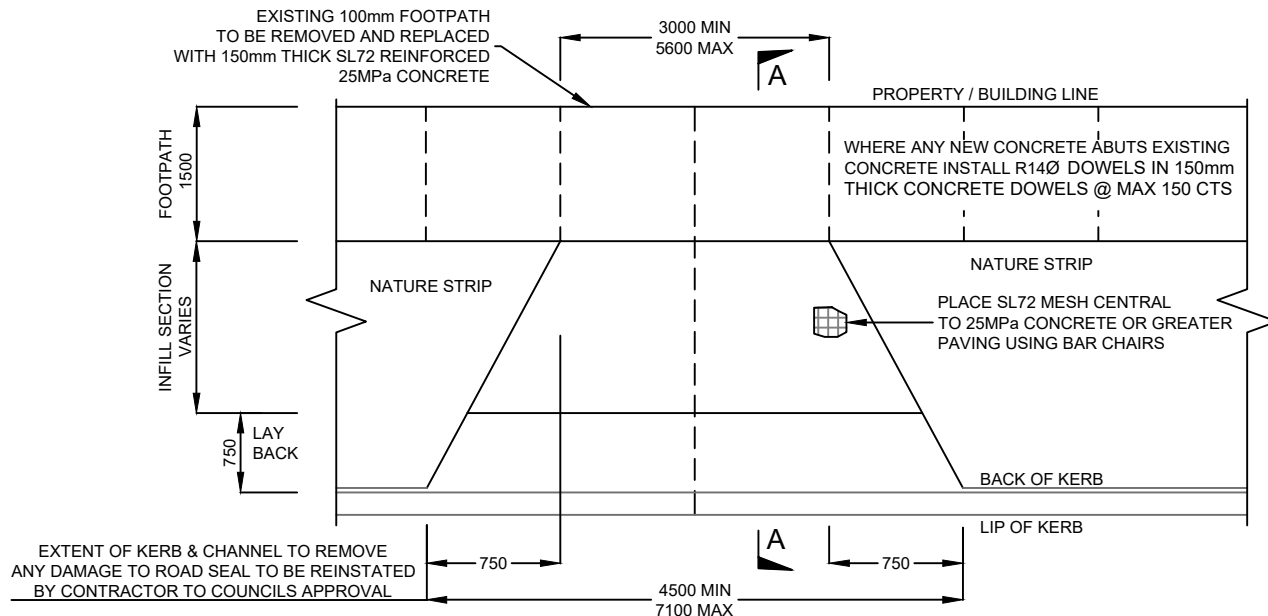
Banyule's Standard Drawings  
A copy can be found on Banyule's website  
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Approved by: James Kelly  
Manager Delivery & Assets

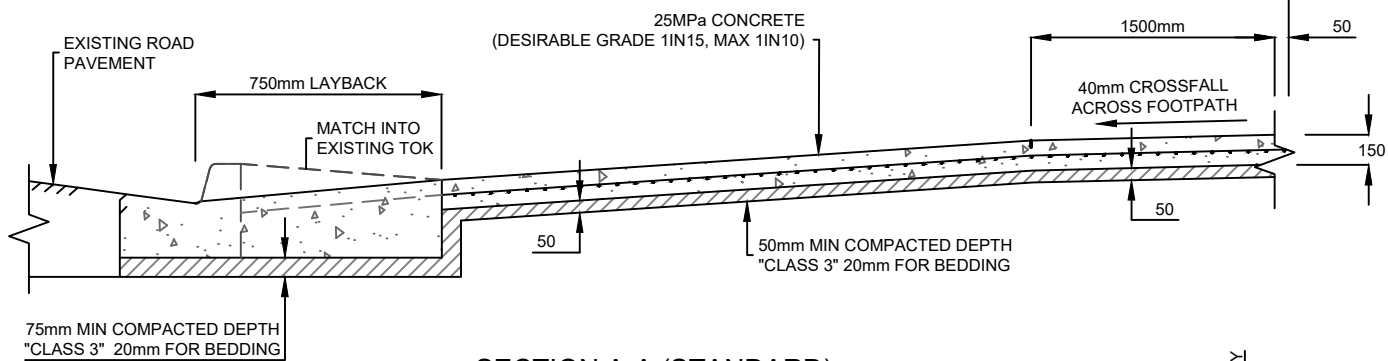
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**R 200**

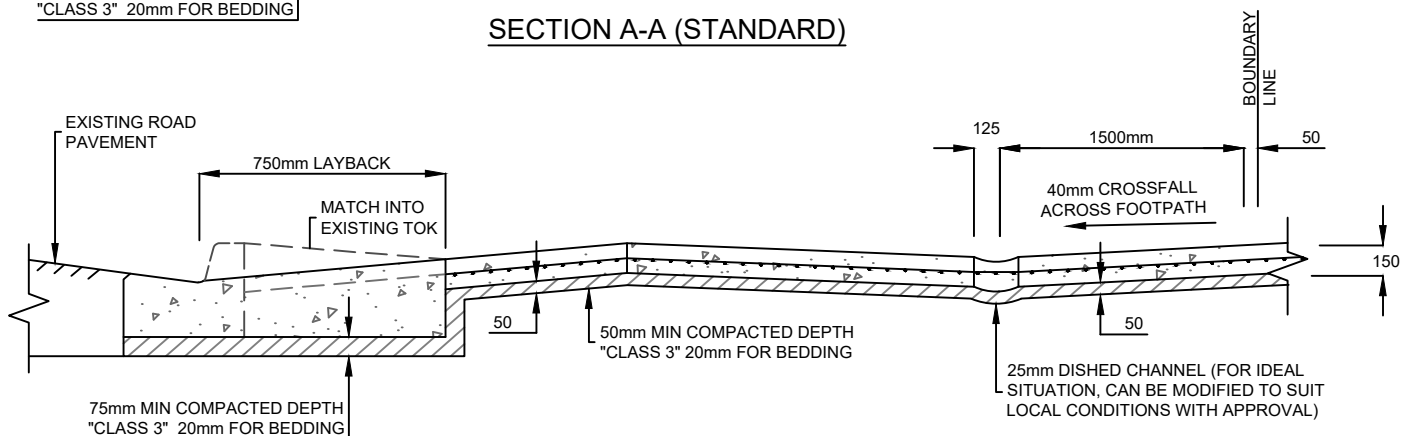
**NOT TO SCALE**



**PLAN**



**SECTION A-A (STANDARD)**



**SECTION A-A (REVERSE FALL)**

(ONLY TO BE USED WITH COUNCILS APPROVAL)

**NOTES:**

1. BULL NOSE MAY BE APPLICABLE FOR GRADES STEEPER THAN 1IN15
2. LAYBACK & CROSSOVER, TO BE CONSTRUCTED IN PLAIN CONCRETE ONLY (NO COLOURED CONCRETE BEYOND PROPERTY BOUNDARY) T.O.K. DENOTES TOP OF KERB.
3. STIPPLE FINISH REQUIRED

**LEGEND:**

CONSTRUCTION JOINTS - - - - -

ALL MEASUREMENTS IN MILLIMETRES



**Banyule**  
CITY COUNCIL

**NEW RESIDENTIAL SINGLE VEHICLE CROSSING DETAIL**

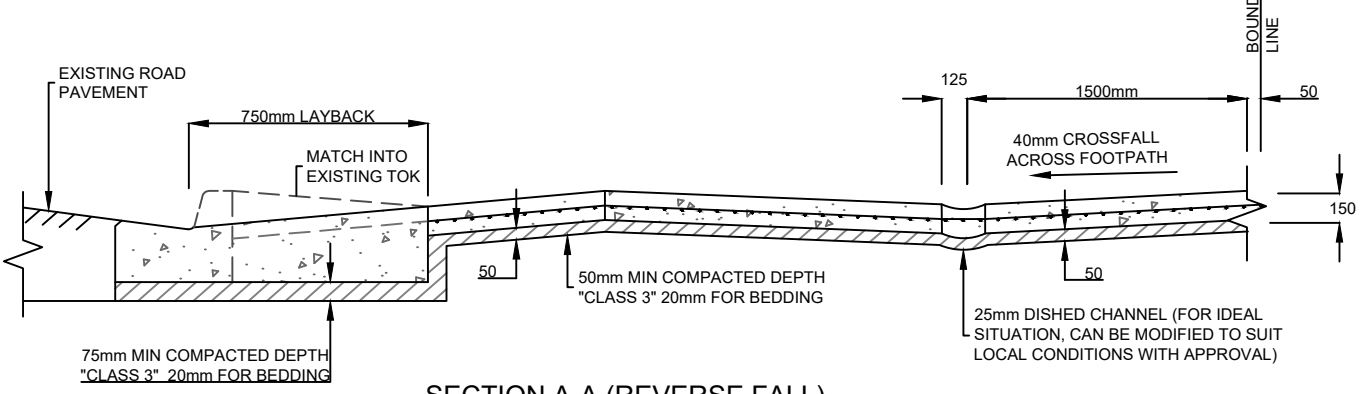
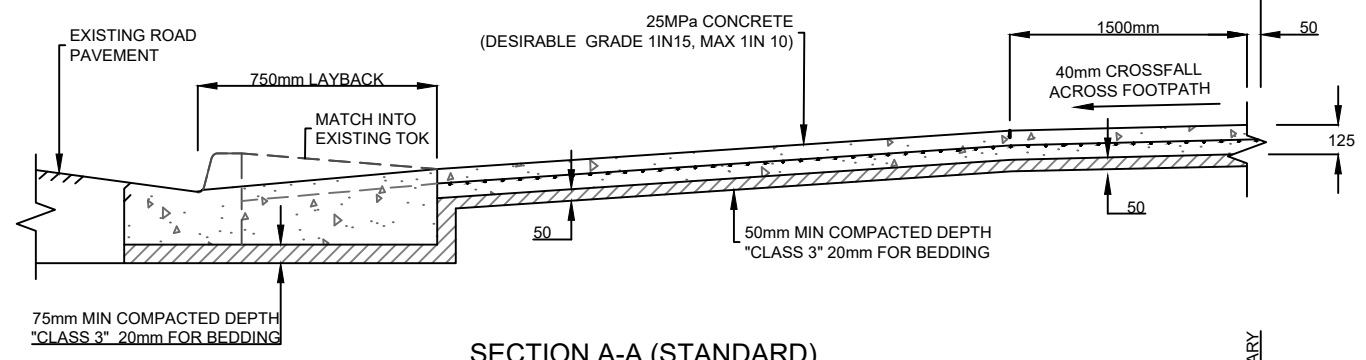
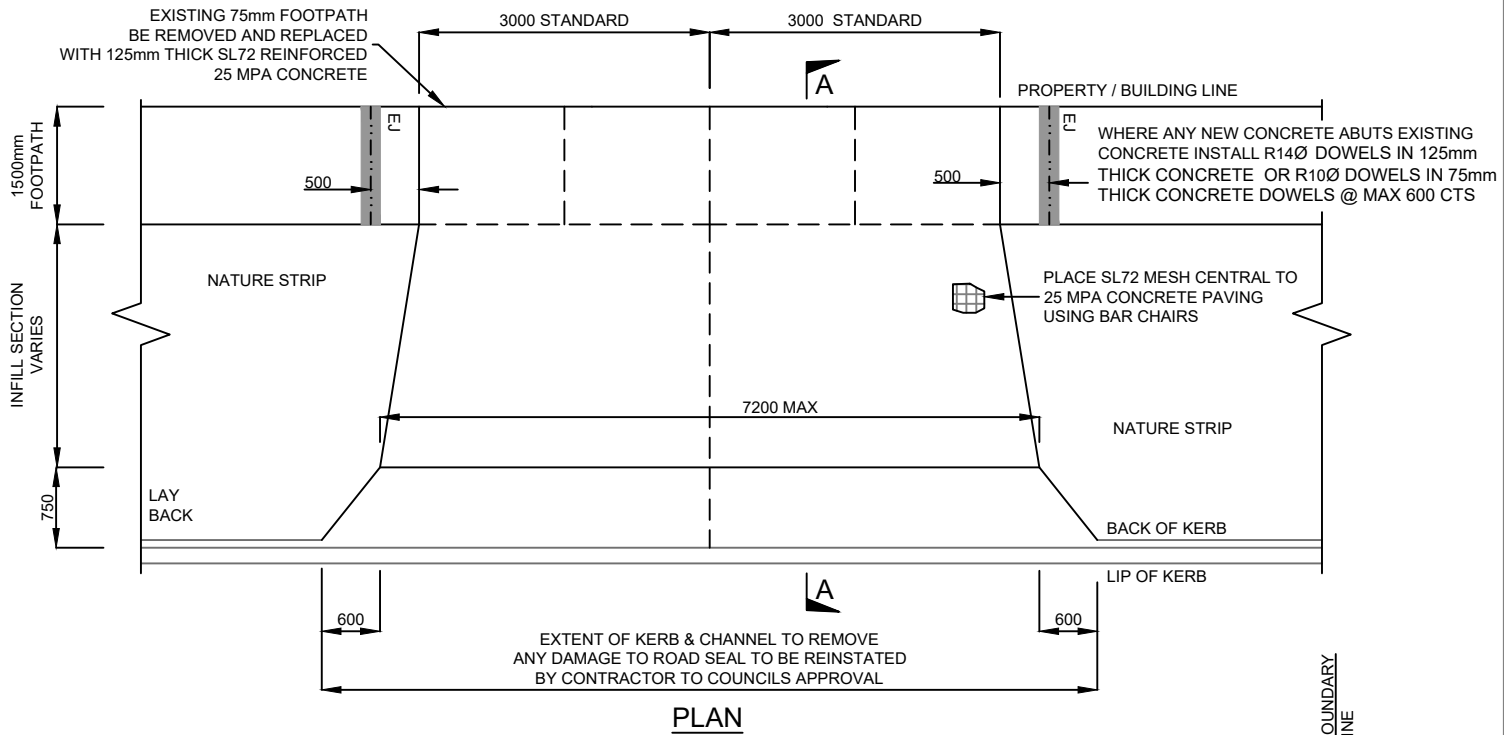
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**R 205**

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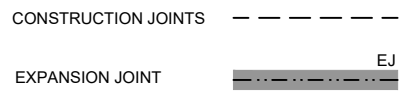


(ONLY TO BE USED WITH COUNCILS' APPROVAL)

**NOTES:**

- LAYBACK & CROSSOVER, TO BE CONSTRUCTED IN PLAIN CONCRETE ONLY (NO COLOURED CONCRETE BEYOND PROPERTY BOUNDARY) T.O.K DENOTES TOP OF KERB.
- REINFORCEMENT MESH TO BE CENTRALLY PLACED.
- 150mm CONCRETE DEPTH WITH BAR CHAIRS.

**LEGEND:**



ALL MEASUREMENTS IN MILLIMETRES



**NEW RESIDENTIAL SHARED / DOUBLE VEHICLE CROSSING DETAILS**

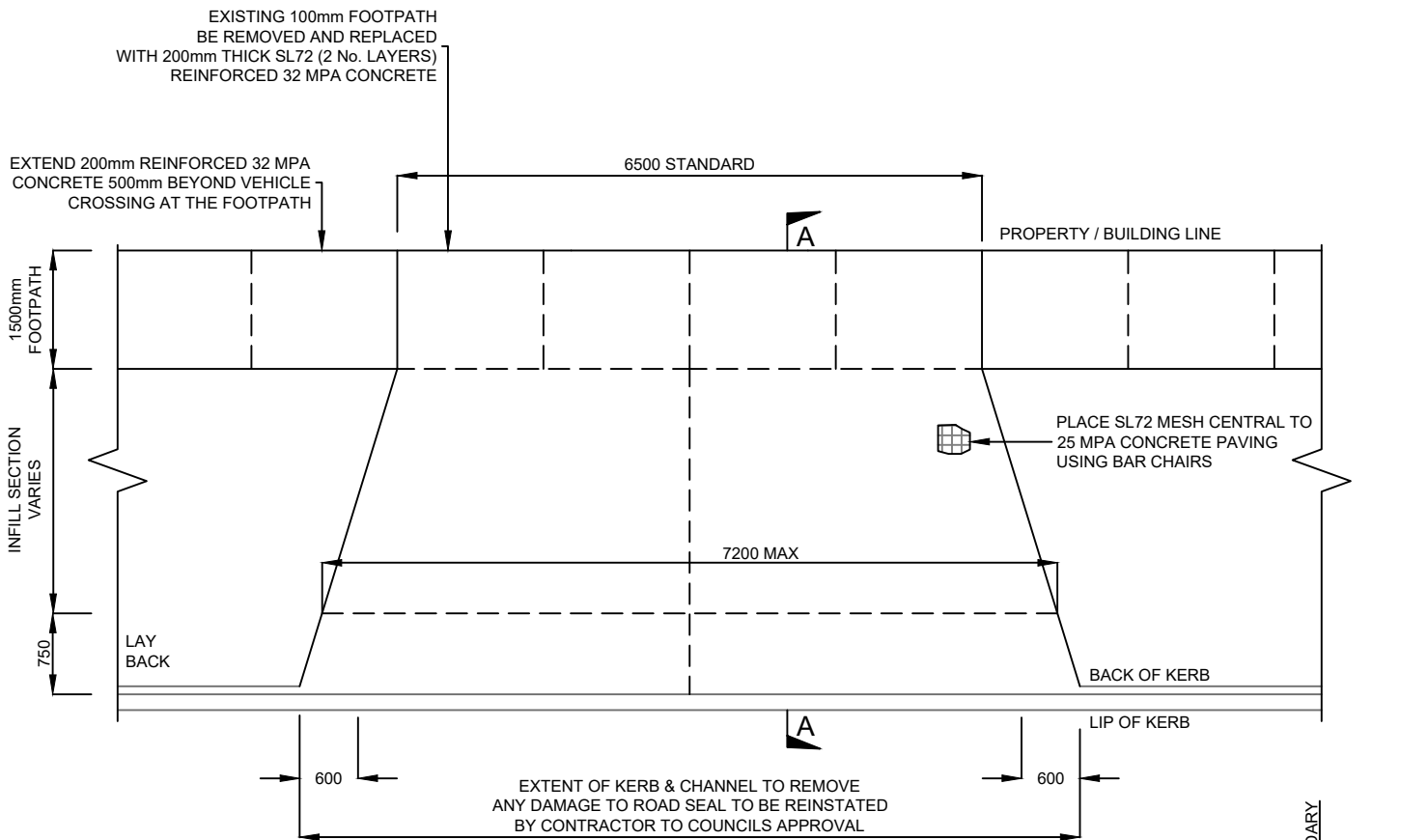
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LAST UPDATED JULY 2019

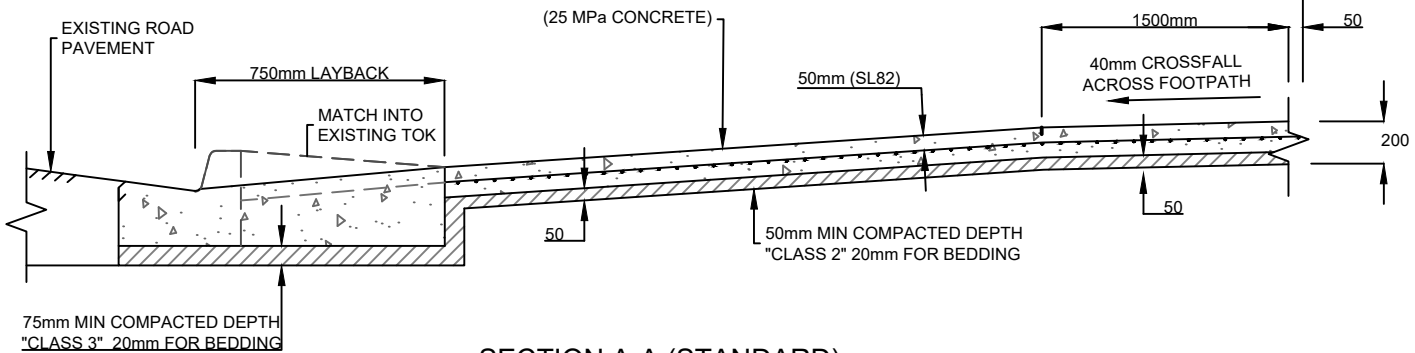
**R 210**

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Manager Delivery & Assets



**PLAN**



**SECTION A-A (STANDARD)**

**NOTES:**

1. T.O.K DENOTES TOP OF KERB
2. TOOL JOINT TO BE DETERMINED ON SITE
3. CONCRETE COVER FOR CROSSOVER 200MM WITH 2xSL72 REINFORCED WITH BAR CHAIRS
4. CROSSFALL GRADE AT 1 IN 10

**LEGEND:**

CONSTRUCTION JOINTS - - - - -

ALL MEASUREMENTS IN MILLIMETRES



**NEW INDUSTRIAL VEHICLE CROSSING DETAIL**

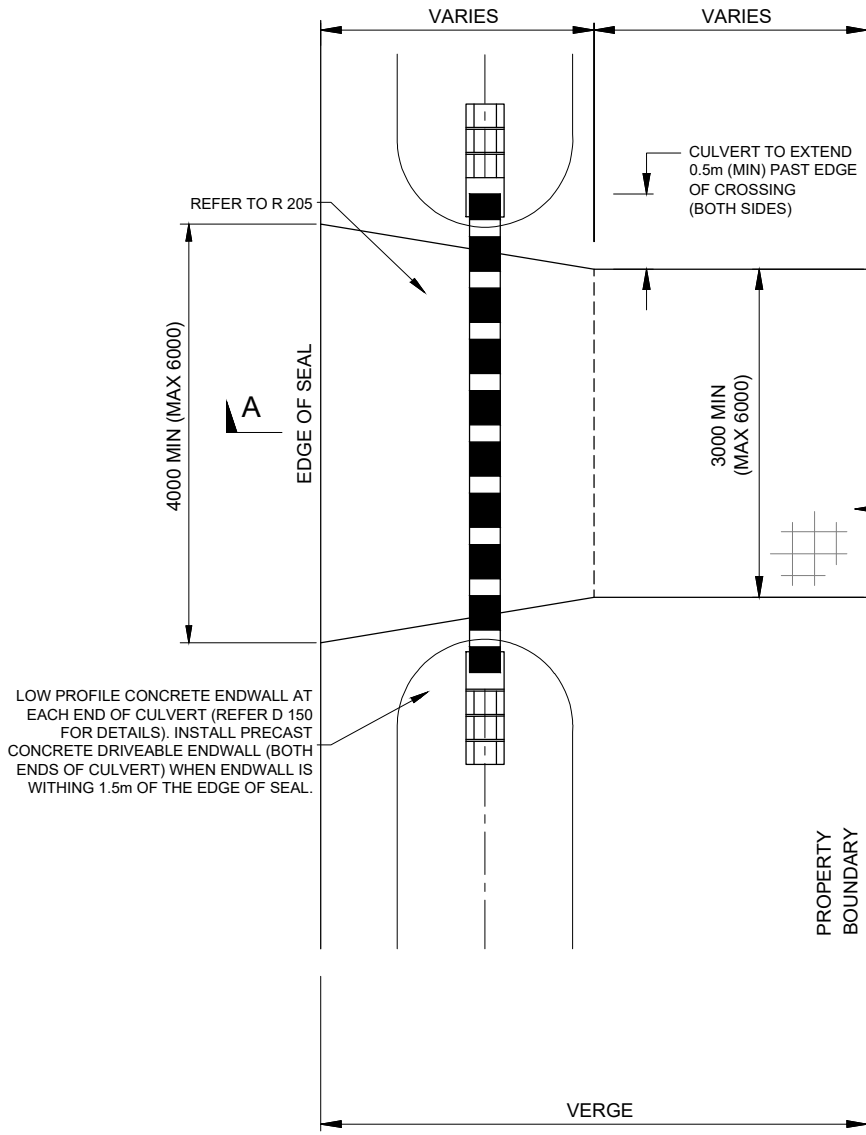
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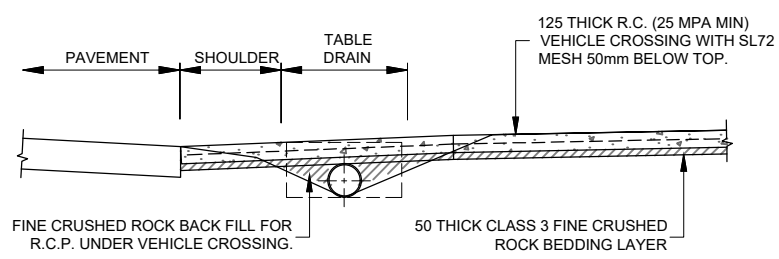
**R 215**

**NOT TO SCALE**



LOW PROFILE CONCRETE ENDWALL AT EACH END OF CULVERT (REFER D 150 FOR DETAILS). INSTALL PRECAST CONCRETE DRIVEABLE ENDWALL (BOTH ENDS OF CULVERT) WHEN ENDWALL IS WITHIN 1.5m OF THE EDGE OF SEAL.

- NOTES:**
1. THIS ARRANGEMENT IS INTENDED FOR RURAL LOW DENSITY RESIDENTIAL ACCESS WAYS.
  2. COUNCIL RESERVES THE RIGHT TO DIRECT THE USE OF CULVERT END WALL TYPE.
  3. THIS IS A TYPICAL CROSSING PLAN. SLIGHT VARIATIONS MAY OCCUR AFTER INSPECTION AND APPROVAL OF LOCATION BY COUNCIL.
  4. PRIOR TO THE CONSTRUCTION, THE CROSSING LOCATION SHALL BE APPROVED BY COUNCIL.
  5. ALL WORKS TO BE COMPLETED TO THE SATISFACTION OF COUNCIL.
  6. MAINTENANCE OF THE CROSSOVER REMAINS THE RESPONSIBILITY OF THE LAND OWNER.
  7. DRIVEABLE ENDWALLS TO BE USED WITHIN 1.5m OF THE EDGE OF SEAL.
  8. FOR ACCURATE CLEAR ZONE GUIDELINES REFER TO AUSTRROADS 'GUIDE TO ROAD DESIGN - PART 6: ROADSIDE DESIGN, SAFETY AND BARRIERS' TABLE 4.1: 'CLEAR ZONES DISTANCES FROM EDGE OF THROUGH TRAVELLED WAY'
  9. TABLE DRAINS ARE NOT TO BE CLOSER THAN 1.0m FROM FENCE LINES OR SERVICES.
  10. CULVERT TO BE LOCATED AT LEAST 600mm FROM EDGE OF SEAL.
  11. PIPE DIAMETER MIN IS 225 OR GREATER PIPE SIZE TO BE DETERMINED BY AUTHORISED OFFICER.
  12. CROSSING PAVEMENT TO BE SEALED WHERE ABUTS A SEALED ROAD.
  13. GATE OFFSET DIMENSIONS:



STANDARD VEHICLE TYPE	MINIMUM GATE OFFSET FROM EDGE OF THROUGH LANE (m)
CAR (5.0m)	8.2
RIGID TRUCK (12.0m)	12.5
SEMI (19.0m)	22
B-DOUBLE (25.0m)	28

ALL MEASUREMENTS IN MILLIMETRES



## TYPICAL SWALE DRAIN VEHICLE CROSSING

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Manager Delivery & Assets

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# R 220

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